

THE CORPORATION OF THE TOWNSHIP OF LANGLEY

TOWNSHIP OF LANGLEY ZONING BYLAW 1987 NO. 2500

AMENDMENT (ISLE OF MANN PROPERTY GROUP)

BYLAW 2020 NO. 5635

EXPLANATORY NOTE

Bylaw 2020 No. 5635 rezones 0.92 ha (2.27 ac) of land at 20701 – 84 Avenue and 8400 Block of 207A Street from Suburban Residential Zone SR-2 and Residential Compact Lot Zone R-CL to Residential Zone R-1A, Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH) and R-CL(SD) to accommodate a mixed residential development consisting of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots).

THE CORPORATION OF THE TOWNSHIP OF LANGLEY

TOWNSHIP OF LANGLEY ZONING BYLAW 1987 NO. 2500

AMENDMENT (ISLE OF MANN PROPERTY GROUP)

BYLAW 2020 NO. 5635

A Bylaw to amend Township of Langley Zoning Bylaw 1987 No. 2500

The Municipal Council of the Corporation of the Township of Langley, in Open Meeting Assembled, ENACTS AS FOLLOWS:

1. This Bylaw may be cited for all purposes as "Township of Langley Zoning Bylaw 1987 No. 2500 Amendment (Isle of Mann Property Group) Bylaw 2020 No. 5635".
2. The "Township of Langley Zoning Bylaw 1987 No. 2500" as amended is further amended by rezoning the lands described as:

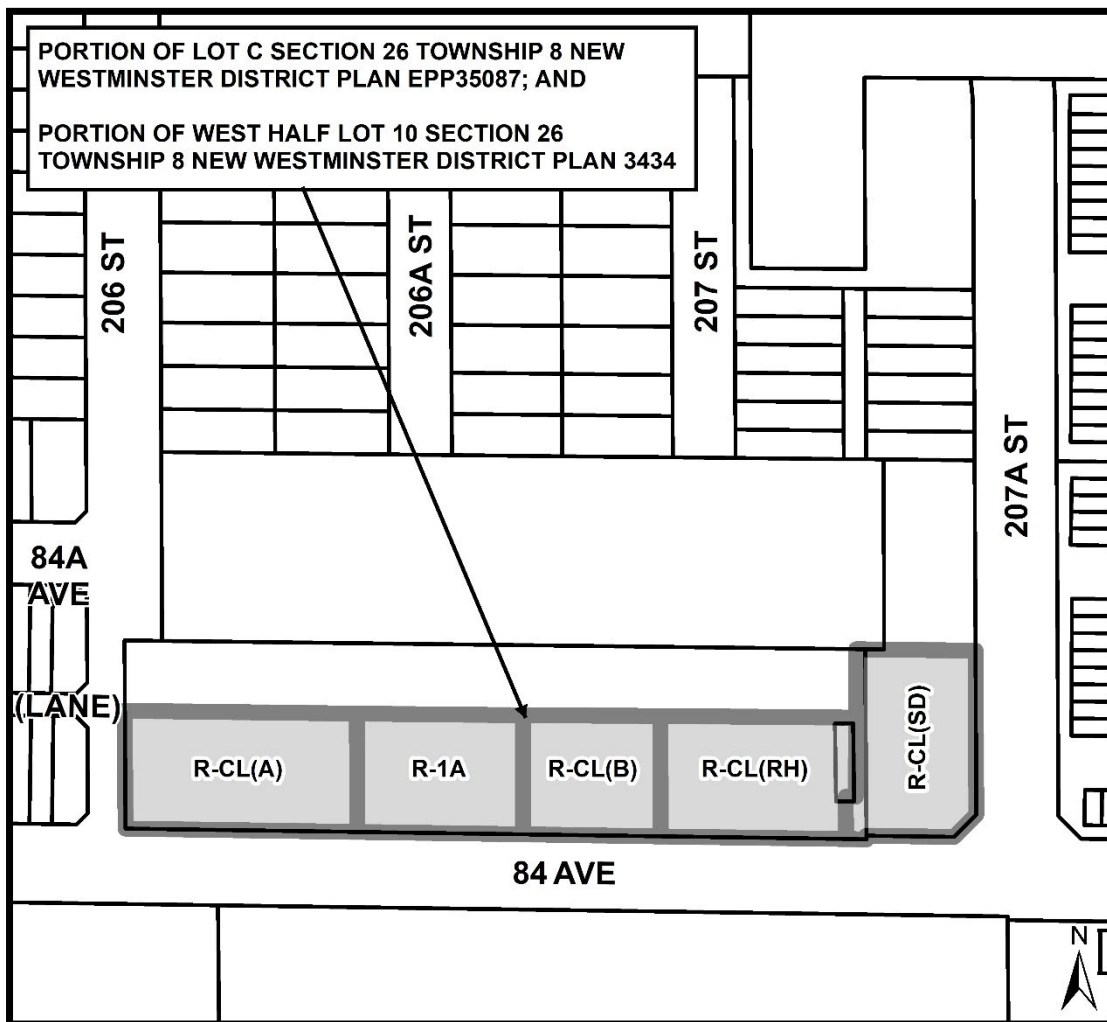
Portion of Lot C Section 26 Township 8 New Westminster District Plan EPP35087;
and
Portion of West Half Lot 10 Section 26 Township 8 New Westminster District Plan 3434

As shown delineated on Schedule "A" attached to and forming part of this Bylaw to Residential Zone R-1A, Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH) and R-CL(SD) as shown on Schedule "A".

READ A FIRST TIME the	05	day of	October	, 2020
READ A SECOND TIME the	05	day of	October	, 2020
NOTICE WAS ADVERTISED the	15, 22	day of	October	, 2020
READ A THIRD TIME the		day of		, 2020
RECEIVED THE APPROVAL OF THE MINISTRY OF TRANSPORTATION AND INFRASTRUCTURE the		day of		, 2020
ADOPTED the		day of		, 2020

_____ Mayor _____ Township Clerk

SCHEDULE 'A' BYLAW NO. 5635



Township of
Langley



Est. 1873

REPORT TO MAYOR AND COUNCIL

PRESENTED: OCTOBER 5, 2020 - REGULAR MEETING
FROM: COMMUNITY DEVELOPMENT DIVISION
SUBJECT: REZONING APPLICATION NO. 100527 AND
DEVELOPMENT PERMIT APPLICATION NO. 101189
(ISLE OF MANN PROPERTY GROUP / 20701 – 84 AVENUE
AND 8400 BLOCK OF 207A STREET)

REPORT: 20-128
FILE: 08-26-0208

PROPOSAL:

Application to rezone approximately 0.92 ha (2.27 ac) of land on portions of 20701 – 84 Avenue and the northwest corner lot at 84 Avenue and 207A Street to Residential Zone R-1A, Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH) and R-CL(SD), to facilitate development of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots).

RECOMMENDATION SUMMARY:

That Council give first and second reading to Bylaw No. 5635 subject to nine (9) development prerequisites being satisfied prior to final reading; that Council authorize issuance (at time of final reading of Bylaw No. 5635) of Development Permit No. 101189; and that staff be authorized to proceed with the written submission opportunity.

RATIONALE:

The proposed development is consistent with the overall objectives of the Willoughby Community Plan and the Yorkson Neighbourhood Plan.



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 DEVELOPMENT PERMIT APPLICATION NO. 101189
 (ISLE OF MANN PROPERTY GROUP / 20701 – 84 AVENUE
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RECOMMENDATIONS:

That Council give first and second reading to Township of Langley Zoning Bylaw 1987 No. 2500 Amendment (Isle of Mann Property Group) Bylaw 2020 No. 5635, rezoning 0.92 ha (2.27 ac) of land located at 20701 – 84 Avenue and 8400 Block of 207A Street to Residential Zone R-1A, Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH) and R-CL(SD), to facilitate the development of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots) subject to the following development prerequisites being satisfied to the acceptance of the Township of Langley General Manager of Engineering and Community Development, unless noted otherwise prior to final reading:

1. A Servicing Agreement being entered into with the Township to secure required road and utility upgrades and extensions in accordance with the Township's Subdivision and Development Servicing Bylaw and Yorkson Engineering Services Plan;
2. Submission of an erosion and sediment control plan in accordance with the Erosion and Sediment Control Bylaw;
3. Provision of road dedications, widenings, and necessary traffic improvements for 206 and 207A Street, and 84 Avenue, in accordance with the Township's Master Transportation Plan, Subdivision and Development Servicing Bylaw and the Yorkson Neighbourhood Plan, and the Street Trees and Boulevard Plantings Policy;
4. Provision of a final tree management plan incorporating tree retention, replacement, protection details, and security in compliance with the Subdivision and Development Servicing Bylaw (Schedule I – Tree Protection);
5. Approval of the rezoning bylaw by the Ministry of Transportation and Infrastructure;
6. Registration of restrictive covenants acceptable to the Township:
 - a. Restricting development of rowhouse and semi-detached lots until a Development Permit is issued for the lots;
 - b. Prohibiting clearing of the rowhouse and semi-detached residential portion of the site (with the exception of servicing access areas) until such time as a final tree management plan incorporating tree retention, replacement, protection details, and security in compliance with the Subdivision and Development Servicing Bylaw (Schedule I – Tree Protection), is accepted by the Township;
 - c. Prohibiting the development of secondary suites and prohibiting garages from being developed for purposes other than parking of vehicles within individual units zoned Residential Compact Lot Zone R-CL(RH) and R-CL(SD);
 - d. Prohibiting clearing and restricting building on proposed Lot 21 until future development in conjunction with 8440 - 206 Street;
 - e. Identifying the units (minimum 5% for single family and semi-detached units) required to incorporate the Adaptable Housing Requirements;
7. Registration of a 6 metre wide statutory right of way on the west portion of "Lot 16" to permit temporary access;
8. Compliance with the Community Amenity Contributions Policy, Willoughby Arterial Road Completion Amenity Policy if applicable, and the requirements of the Yorkson Greenway Amenity Policy;
9. Payment of applicable Neighbourhood Planning Administration fees, supplemental Rezoning fees, Site Servicing Review fee, ISDC review fee, Development Works Agreement (DWA) and Latecomer charges, and compliance with the Township's 5% Neighbourhood Park Land Acquisition Policy;

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That Council at time of final reading of Rezoning Bylaw No. 5635 authorize issuance of Development Permit No. 101189 (proposed single family lots) subject to the following conditions:

- a. An exterior design control agreement shall be entered into for all lands zoned Residential Zone R-1A and Residential Compact Lot Zones R-CL(A) and R-CL(B) ensuring that building design and site development standards are high quality, consistent and compatible with other lots and development in accordance with Section 4.2.1 of the Yorkson Neighbourhood Plan;
- b. Written confirmation from owner and landscape architect or arborist that the tree protection fencing identified in the tree management plan is in place; and
- c. Payment of supplemental development permit fees prior to issuance.

That Council authorize staff to proceed with the written submission opportunity notice prior to Council's consideration of third reading of Rezoning Bylaw No. 5635 in conjunction with Development Permit No. 101189.

EXECUTIVE SUMMARY:

Isle of Mann Property Group has applied to rezone a 0.92 ha (2.27 ac) site located at 20701 – 84 Avenue and the lot at the northwest corner of 84 Avenue and 207A Street to Residential Zone R-1A, and Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH), and R-CL(SD) to facilitate development of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots).

Development Permit No. 101189 is being processed in conjunction with the rezoning application to secure an exterior design control agreement for the R-1A, R-CL(A), and R-CL(B) zoned lots at the time of subdivision. Future Development Permits (yet to be applied for) will provide Council with the opportunity to review the form, character and siting of the rowhouses and semi-detached lots. Typically these are presented to Council concurrently with the Rezoning Bylaw for efficiency and to allow Council as well as the public an opportunity to evaluate the rezoning and the form and character at the same time. However, the applicant has requested to advance the rezoning bylaw for consideration without the accompanying form and character development permit at this time.

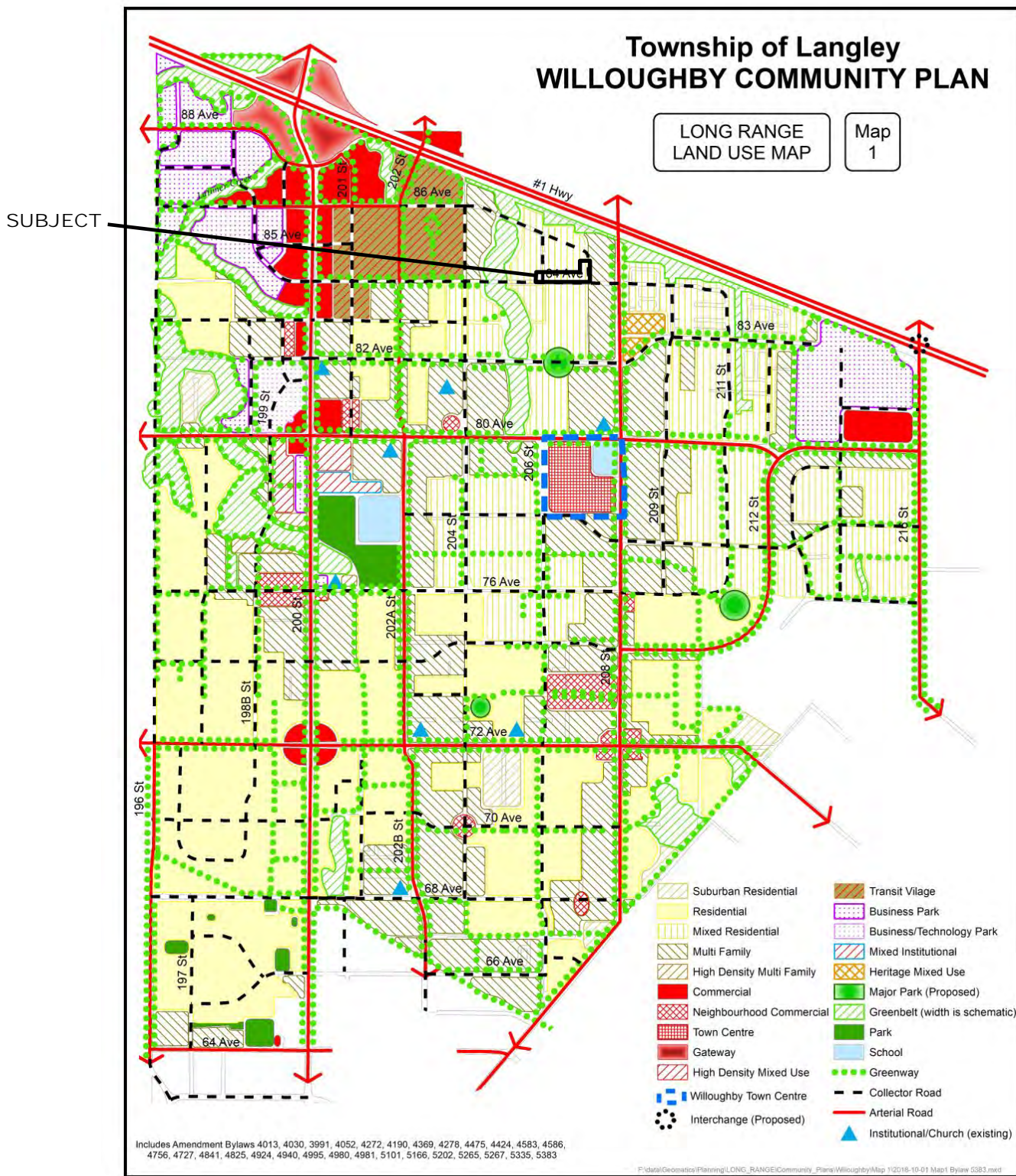
The proposed rezoning is consistent with the overall objectives of the Willoughby Community Plan and the Yorkson Neighbourhood Plan. Staff recommend that Council consider the rezoning request, subject to the completion of nine (9) development prerequisites. Staff also recommend that Council authorize issuance (at time of final reading of Bylaw No. 5635) of Development Permit No. 101189 (proposed single family lots).

PURPOSE:

The purpose of this report is to advise and make recommendations to Council with respect to Rezoning Bylaw No. 5635 and Development Permit No. 101189.

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REFERENCE:

Owner:	1124300 BC Ltd. 401 – 15336 – 31 Avenue Surrey BC V3Z 0X2
Agent:	Isle of Mann Property Group 401 – 15336 – 31 Avenue Surrey BC V3Z 0X2
Legal Description:	Portion of Lot C Section 26 Township 8 New Westminster District Plan EPP35087 Portion of West Half Lot 10 Section 26 Township 8 New Westminster District Plan 3434
Civic Address:	20701 – 84 Avenue and 8400 Block of 207A Street
Area:	0.92 ha (2.27 ac)
Willoughby Community Plan:	Mixed Residential
Yorkson Neighbourhood Plan:	Mixed Residential (20 – 25 uph (8 – 10 upa))
Existing Zoning:	Suburban Residential Zone SR-2 (0.8 ha / 2.0 ac minimum lot size); and Residential Compact Lot Zone R-CL
Proposed Zoning:	Residential Zone R-1A; Residential Compact Lot Zone R-CL(A); Residential Compact Lot Zone R-CL(B); Residential Compact Lot Zone R-CL(RH); and Residential Compact Lot Zone R-CL(SD)

BACKGROUND/HISTORY:

The subject lands are comprised of two lots currently zoned Suburban Residential Zone SR-2 and Residential Compact Lot Zone R-CL while designated “Mixed Residential” (20 – 25 units per hectare / 8 – 10 units per acre) in the Yorkson Neighbourhood Plan. The area under application consists of two (2) parcels. The western property (20701 – 84 Avenue) is currently zoned Suburban Residential Zone SR-2 and used for suburban residential purposes while the eastern property is currently vacant and split zoned Suburban Residential Zone SR-2 and Residential Compact Lot Zone R-CL. The northern portion was rezoned to Residential Compact Lot Zone R-CL in 2005 as part of ToL Project No. 08-25-0059 while the southern portion is currently zoned Suburban Residential Zone SR-2.

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DISCUSSION/ANALYSIS:

Isle of Mann Property Group has applied to rezone 0.92 ha (2.27 ac) of land in the Yorkson Neighbourhood Plan North Phase from Suburban Residential Zone SR-2 and Residential Compact Lot Zone R-CL to Residential Zone R-1A, Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(SD) and R-CL(RH). The proposed rezoning will facilitate a mixed residential development consisting of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots).

A Development Permit for the single family detached lots is being processed in conjunction with the application. Although the applicant has requested not to apply for development permits for the rowhouses at this time, however, would need to before they can commence building on these lots.

As noted on the preliminary subdivision plan, the north portion is not proposed to be rezoned at this time. As a condition of rezoning the site, a no-build restrictive covenant is required to be registered on this portion of land until such time as they can be developed in conjunction with the adjacent property. Staff note that a rezoning / subdivision application (TOL Project #08-26-0214) is currently being processed on the adjacent site to the north.

Adjacent Uses:

North: Property zoned Suburban Residential Zone SR-2, designated Mixed Residential (20 – 25 uph / 8 – 10 upa) in the Yorkson Neighbourhood Plan and currently under application (ToL Project No. 08-26-0214);

South: 84 Avenue, beyond which is property zoned Suburban Residential Zone SR-2, designated Mixed Residential (20 – 25 uph / 8 – 10 upa) in the Yorkson Neighbourhood Plan and Yorkson Creek Middle School;

East: 207A Street, beyond which is a townhouse development zoned Comprehensive Development Zone CD-92 and designated Townhouse (37 – 62 uph / 15 – 25 upa) in the Yorkson Neighbourhood Plan; and

West: 206 Street, beyond which are lots zoned Residential Compact Lot Zone R-CL(RH) and designated Mixed Residential (20 – 25 uph / 8 – 10 upa) in the Yorkson Neighbourhood Plan.

Zoning Amendment:

The subject development site is currently zoned Suburban Residential Zone SR-2 and Residential Compact Lot Zone R-CL. Bylaw No. 5635 proposes to rezone a portion of the site Residential Zone R-1A and Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH), and R-CL(SD). The rezoning will facilitate development of a mixed residential development consisting of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots).

Public Consultation:

As per Policy No. 07-164, the applicant held a public meeting on October 4, 2018. Results of the Developer Held Public Information Meeting were compiled and are provided on Attachment D.

Development Permit:

All of the Yorkson Neighbourhood Plan area is designated as a mandatory Development Permit area in the Willoughby Community Plan. The subject site is located in “Development Permit Area ‘B’ – Residential” which establishes objectives and guidelines for the form, character and siting of residential development. The primary objective of the development permit area

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designation is to encourage the development of attractive and safe residential areas and reduce conflict with adjacent land uses.

The specific development permit area guidelines outlined in the Plan related to single family residential development are attached to this report (Attachment B). For the single family residential lots, the Development Permit guidelines are implemented by a Development Permit requirement that the proponent enter into an exterior design control agreement at the time of subdivision. The exterior design control agreement typically addresses the form, character and siting of individual single family homes constructed in a development.

A future Development Permit application is required for the attached residential component of the development (rowhomes and semi-detached lots) at which time Council will have an opportunity to review form, character and siting matters prior to building permits being issued. The Development Permit typically details the form and character of each development including siting, access, parking, landscaping, building materials, colours, building elevations and other site development items. As a condition of final reading, the proponent must register a restrictive covenant prohibiting development of the attached residential portion of the development until such time as a Development Permit has been issued.

Subdivision:

Preliminary subdivision plans have been submitted in support of the rezoning application. Details of the subdivision will be addressed at the subdivision stage pursuant to the requirements of the Subdivision and Development Servicing Bylaw.

Adaptable Housing:

In accordance with Section 4.2.1(9) of the Yorkson Neighbourhood Plan a minimum of 5% of the single family and townhouse units in any residential development shall provide adaptable housing. Council has chosen to implement this provision through the adoption and implementation of the Adaptable Housing Requirements. The adaptable units are to be identified and secured by restrictive covenant prior to final reading.

School Sites:

School District 35 has provided comments (Attachment C) and anticipates that the overall proposed development upon completion of all phases will generate approximately eight (8) new students for Willoughby Elementary (located approximately 850 metres south of the site), three (3) new students for Yorkson Creek Middle School (located approximately 30 metres south of the site) and six (6) students for RE Mountain Secondary School (located approximately 1.7 kilometres south of the site).

Parks:

The closest existing park is Yorkson Community Park located approximately 50 metres to the south of the site.

Community Amenity Contributions:

Staff note that the Community Amenity Contributions (CAC) Policy (adopted by Council on July 23, 2018 and subsequently revised on April 15, 2019 and November 18, 2019) is applicable to the subject residential rezoning application. The policy specifies target contribution amounts based on unit types. The applicant has reviewed the policy and has agreed to provide the

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current target contribution amounts specific to this development, which are \$6,524 per single family lot and \$5,536 per rowhouse / duplex unit for a total of \$126,136.

Tree Protection/Replacement:

The tree management plan submitted by the applicant indicates that five (5) significant trees exist on the overall site of which one (1) is within the proposed current development area, three (3) are on proposed lot 21 (intended for future development) and one (1) is within the rowhouses and semi-detached lots that will be subject to future development permits. The one (1) significant tree in the current development area is not proposed for retention. As the applicant has requested to advance this rezoning application without the accompanying form and character development permit for the proposed rowhouses and semi-detached lots, a condition of rezoning is the registration of a restrictive covenant prohibiting clearing (with the exception of servicing access) until such a time as a development permit is issued. Subsequent tree management plans are required as part of future applications.

For the current single family lots proposed 38 replacement trees are required. Additionally, approximately 24 street trees are required along the road frontages (in compliance with the Township's Street Tree and Boulevard Plantings Policy). Final tree retention, protection, and replacement plans are subject to the final acceptance of the Township. This requirement has been included in the list of development prerequisites to be completed prior to final reading of the rezoning bylaw.

Servicing:

Prior to final reading, the applicant is required to enter into a Servicing Agreement to secure works and services such as construction of road works, tree replacement and utility upgrades and/or extensions in accordance with the Subdivision and Development Servicing Bylaw and the Yorkson Neighbourhood Plan to the acceptance of the Township. The applicant will also be required to provide erosion and sediment control measures in accordance with the Erosion and Sediment Control Bylaw, to the acceptance of the Township.

Environmental Considerations:

The Township's Sustainability Charter includes environmental objectives to protect and enhance rivers, streams, wildlife habitats and environmentally sensitive areas in the Township. These environmental objectives are supported by policy and guidance outlined in the Township's Environmentally Sensitive Areas Study, Wildlife Habitat Conservation Strategy, Schedule 3 of the OCP, Erosion and Sediment Control Bylaw, and Subdivision and Development Servicing Bylaw (Schedule I – Tree Protection) which promote sound environmental management practices and outline Township environmental performance expectations. The provision of stormwater management and sediment control measures and compliance with the Township's Subdivision and Development Servicing Bylaw (Schedule I – Tree Protection) satisfies the objectives of the Sustainability Charter.

Transit:

Currently, transit service is provided along 208 Street (approximately 200 metres from the subject site) via the 595 bus.

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Intergovernmental Implications:

As the subject site is located within 800 metres of a controlled access highway (Highway No. 1), approval of the rezoning bylaw by the Ministry of Transportation and Infrastructure is required prior to Council's consideration of final reading.

POLICY CONSIDERATIONS:

The proposed rezoning and development permit will facilitate the rezoning and subdivision of 21 lots (10 single family lots, 5 rowhouses and 6 semi-detached lots). The proposed rezoning complies with the land use and density provisions of the Willoughby Community Plan, the Yorkson Neighbourhood Plan and with the proposed Residential Zone R-1A and Residential Compact Lot Zones R-CL(A), R-CL(B), R-CL(RH), and R-CL(SD).

Staff recommend that Council give first and second reading to Bylaw No. 5635 (subject to nine (9) development prerequisites) and authorize issuance (at time of final reading of Bylaw No. 5635) of accompanying Development Permit No. 101189 and authorize staff to schedule the written submission opportunity.

Respectfully submitted,

Joel Nagtegaal
DEVELOPMENT PLANNER
for
COMMUNITY DEVELOPMENT DIVISION

ATTACHMENT A	Development Permit No.101189
ATTACHMENT B	Willoughby Development Permit Area B – Residential Guidelines
ATTACHMENT C	School District 35 comments
ATTACHMENT D	Public Information Meeting Summary

THE CORPORATION OF THE TOWNSHIP OF LANGLEY

Development Permit No. 101189

This Permit is issued this _____ day of _____, 2020 to:

1. Name: 1124300 BC Ltd.

Address: 401 – 15336 – 31 Avenue
Surrey BC V3Z 0X2

2. This permit applies to and only to those lands within the Municipality described as follows and to any and all buildings, structures and other development thereon:

LEGAL DESCRIPTION: Portion of Lot C Section 26 Township 8 New Westminster District
Plan EPP35087

Portion of West Half Lot 10 Section 26 Township 8 New
Westminster District Plan 3434

CIVIC ADDRESS: 20701 – 84 Avenue and 8400 Block of 207A Street

3. This Permit is issued subject to compliance with all of the Bylaws of the Municipality of Langley applicable thereto, except as specifically varied or supplemented by this permit and subject to the following conditions being satisfied to the acceptance of the General Manager of Engineering and Community Development:
- a. An exterior design control agreement shall be entered into for all lands zoned Residential Zone R-1A and Residential Compact Lot Zones R-CL(A) and R-CL(B) ensuring that building design and site development standards are high quality, consistent and compatible with other lots and development in accordance with Section 4.2.1 of the Yorkson Neighbourhood Plan
 - b. Written confirmation from owner and landscape architect or arborist that the tree protection fencing identified in the tree management plan is in place; and
 - c. Payment of supplemental development permit fees prior to issuance.
4. The land described herein shall be developed strictly in accordance with the terms, conditions and provisions of this Permit and any plans and specifications attached as a Schedule to this Permit which shall form a part hereof.

This Permit is not a Building Permit.

All developments forming part of this Development Permit shall be substantially commenced within two years after the date the Development Permit is issued.

This permit shall have the force and effect of a restrictive covenant running with the land and shall come into force on the date of an authorizing resolution passed by Council.

It is understood and agreed that the Municipality has made no representations, covenants, warranties, guarantees, promises or agreement (verbal or otherwise) with the developer other than those in this Permit.

This Permit shall enure to the benefit of and be binding upon the parties hereto and their respective heirs, executors, administrators, successors and assigns.

AUTHORIZING RESOLUTION PASSED BY COUNCIL THIS ____ DAY OF _____, 2020.

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4.1.2 DEVELOPMENT PERMIT AREA “B” - RESIDENTIAL

Lands identified as “Residential” on Map 4, Development Permit Areas are hereby designated as development permit areas under Section 488(1)(e) and (f) of the Local Government Act to establish objectives and provide guidelines for the form and character of intensive and multi family residential development.

The objective of this development permit area designation is to encourage development of attractive and safe multi family areas.

Unless the owner first obtains a development permit, land within this development permit area shall not be subdivided, and construction of, addition to or alteration of a multi family dwelling (including a townhouse, rowhouse, apartment, duplex, triplex or fourplex) must not be started.

Development permit guidelines are as follows:

4.1.2.1 GENERAL

The following general guidelines apply to all development within Development Permit Area “B.”

4.1.2.2 SINGLE FAMILY DEVELOPMENT

General

- Single family developments shall enter into an Exterior Design Control Agreement (to be registered on title as a restrictive covenant) prior to final subdivision approval and to the acceptance of the Township. The agreement shall incorporate the following single family development permit guidelines.

Architectural Details

- No residential units shall back onto a public road or street greenway other than 212 Street between 76 and 80 Avenues.
- All building elevations visible from public land (i.e. parks, roads, greenways and detention pond sites) shall provide architectural detailing to be consistent with the front of the building.

Parking and Traffic/Pedestrian Circulation

- Where single-family lots abut an arterial road or a street greenway vehicular access and parking shall be provided via a rear lane or any other vehicular access from the rear of the property while retaining the front pedestrian access of the building facing the street.

Landscaping

- Fences adjacent to a street greenway shall not exceed 122 cm (48 inches) in height. These fences shall be designed to complement the building and be an open picket fence design. Fences must permit observation of the public realm and incorporate landscaping to soften their appearance from the road.

Building Form

- Pitched roofs are required. Pitched roofs shall have architectural grade roof material, including ridge caps and shadow lines.

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4.1.2.3 MULTIPLE UNIT DEVELOPMENT

General

The following guidelines apply to all multiple-unit development including but not limited to apartment, townhouse, rowhouse, duplex, triplex and fourplex buildings.

Site Design

- While providing individual design character, buildings shall be designed to integrate and complement adjacent developments with respect to siting, setbacks, design, exterior finish, landscaping and parking areas. Facade and roofline articulation with porches and other projecting elements is required. Blank or undifferentiated facades shall be avoided.
- Buildings shall be sited and designed to maximize sun penetration to adjacent roads, sidewalks and properties.
- Buildings sited on corners shall address both street edges, shall express a visually stimulating 'landmark' architecture, and be massed to define the intersection.
- On sloping sites, buildings should be massed to create a terraced form of development and provide view opportunities for a majority of housing units.
- Site planning and landscaping for residential development should take into account established principles of Crime Prevention Through Environmental Design (CPTED) – including opportunities for neighbourhood surveillance of pathways, landscaped areas and roadways and provision of defensible space that is clearly separated by fences, landscaping or paving, readily visible by residents and adequately lit.
- In order to allow for stormwater infiltration to maintain flow in watercourses, development is encouraged to maintain low surface imperviousness through compact building form and site layout, consideration shall be given to alternative stormwater and road standards, use of pervious surface materials where feasible and preservation of existing vegetation.
- Multi family buildings shall be designed to maximize avoidance of leaky condominium syndrome by using industry best building practices.
- Mail box kiosks located within a stratified development shall be protected from the weather, be architecturally integrated into the development and be located adjacent to a visitor parking stall with pull-out.
- Presenting garages to public roads is discouraged. Offsetting garages behind the front face of the building is encouraged. Carports are not permitted. Developments shall register a restrictive covenant on title preventing conversion of the garage to any other use that prohibits vehicle storage.
- Development of street facing buildings (i.e. the front door is facing towards the municipal roadway) is required abutting a street or street greenway, other than 200 and 212 (between 76 and 80 Avenues) Streets.
- A pedestrian connection shall be provided from each development site to adjacent streets, street greenways, or public spaces. Public, semi-public and private space shall be clearly delineated.
- A strong street presence is required through inclusion of elements such as extended porches and patios, recessed entries, ground oriented units with direct pedestrian street access, and other similar arrangements. Where individual street access to residential units is not practical, building design should foster a relationship with the adjacent street and pedestrians using the street.
- Buildings should be oriented to streets, greenways, or other public spaces, neither gated nor turning away from the public realm, to provide overview for safety and encourage resident involvement with the activities of the neighbourhood.
- Pedestrian street access to individual residential units is strongly encouraged in order to reinforce pedestrian activity and street life.
- Private outdoor spaces of residential buildings fronting public streets shall provide a sense of separation while still contributing to the streetscape. Semi-private outdoor spaces adjacent to the public realm shall be similarly arranged.
- Private driveway access over greenways should be consolidated and minimized to ensure maximum safety of the users of the greenway. Private driveways may be restricted to laneway access only.

Building Form

- Roofscape is an important element of building design. Green roofs and green walls are encouraged in compliance with the BC Building Code. Roofs may also be developed to provide resident amenity. Open areas of flat roofs shall be finished with pavers or other coloured materials to enhance the view from above. Pitched roofs are required unless a green roof or amenity space incorporating landscaping is provided. Flat roofs shall be designed to enhance the view from adjacent buildings with patterned, textured and/or coloured materials and also include activity areas and or green roofs. Low albedo (light coloured) roofing should be used to the greatest possible extent consistent with appearance from above and avoidance of glare from light reflection for the visual comfort of occupants.
- Ground level and roof areas created by setbacks shall be used as active outdoor space wherever possible, arranged to create 'eyes on the street', and appropriately landscaped.
- Building entrances should be clearly identified by the architecture of the building and include articulation or added elements to provide weather protection.

Exterior Design and Finish

- The main entrance of the building should be clearly identified by the architecture of the building and include such elements as pedestrian awnings, canopies, and building overhangs to provide protection from the weather.
- Exterior finish of buildings shall be high quality to ensure integrity of the building envelope design, and to present an attractive appearance.
- Exterior materials, colours and textures shall be selected and applied in the context of newer residential and mixed use developments as well as overall community character.
- Acceptable wall cladding materials include natural and manufactured stone, brick masonry, wood, fiber cement composite siding and panels, metal, and glass. Vinyl as a secondary material is permitted, however, a variety of cladding orientation, material, design and/or colour shall be used. Stucco cladding materials may also be used, however are discouraged, and may not fill more than 25% of any wall surface.
- Glass elements incorporated into weather protection shall be frosted, or provided with other translucent finish, to maintain acceptable appearance between maintenance cycles.
- Mechanical equipment shall be screened or integrated with the roof form, as viewed from the street or higher buildings, in a manner consistent with the overall architecture of the building.
- To provide visual interest elevations of buildings facing a street shall have architectural details such as roofline height, varied colour treatments, windows, articulation in the building envelope, etc.
- Building elevations that are visible from adjacent roads, municipal greenway or other public spaces shall be designed with the same level of care and attention in terms of character, articulation, fenestration, architectural detail, and material quality.
- All exposed base supports for structures that include signage, amenity features, building appurtenances and other site elements shall be architecturally integrated into the overall site design. Unadorned concrete and metal is not permitted.

Landscaping

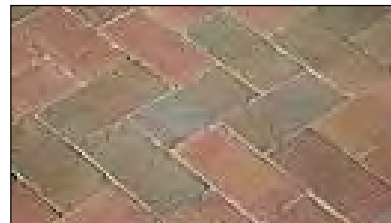
- Significant tree stands and tree corridors shall be incorporated into the development. A Tree Management Plan shall be prepared and submitted in compliance with the Subdivision and Development Servicing Bylaw 2011 No. 4861 (Schedule I – Tree Protection), as amended from time to time.

- Roof top patios shall be landscaped with water and electrical outlets.
- On-site landscaping shall be required to enhance the appearance of the development, screen parking, loading and utility areas, and garbage containers/enclosures from adjacent properties and roadways. Best efforts should be made to appropriately screen all utility boxes and meters.
- A landscape plan shall be prepared by a registered B.C. Landscape Architect.
- Playground facilities shall be provided, in accordance with the Township's Child Friendly Amenity Area requirements, as amended.
- Entrances shall be articulated with appropriate low fencing and high quality features to provide distinction between public and private space.
- Where lots abut municipal property (i.e. environmental area or a park) a black coated chain link fence shall be constructed to municipal standard. If an adequately landscaped buffer (native plant species are encouraged) of at least 2 metres in width is provided on the greenway side of the fence to the acceptance of the Township, other fence types may be used provided they are visually permeable above 122 cm (48 inches) and do not exceed 180cm (6 feet) in total height.
- If security fencing is required for storage areas, black coated chain link fencing screened with hedging material may be used.
- A 5 metre wide landscaping area and a fence shall be provided on multi family properties along abutting lots designated for non residential development (other than municipal greenspace). Fences should be aesthetically designed and reflect adjacent residential building character where applicable.
- The use of perimeter berms (in most circumstances), high fences and security gates is not permitted to provide surveillance and a more pedestrian-friendly street system. Fences adjacent to a public road allowance or a street greenway shall not exceed 122 cm (48 inches) in height. These fences shall complement the building in terms of design character, materials, and colour. Fences must permit observation of the public realm and incorporate landscaping to soften their appearance from the road.
- Landscape materials and design, on development sites and within road right-of-ways and other public spaces, shall be selected in the context of adjacent developments as well as overall community character, all in consultation with the Township.
- Landscape site planning and design shall incorporate both hard and soft materials in support of the principles of CPTED.
- Where fencing or guards are used for life safety purposes or public/private space definition within the landscape, such enclosures shall be as transparent as possible in support of CPTED objectives.
- All retaining walls shall be composed of split face concrete block, natural stone, or patterned cast-in-place concrete. Material selection shall be in the context of, and integrate with similar installations on adjacent properties.
- The public realm shall incorporate street furniture and amenities, heritage artifacts, and public art pieces, to enhance the pedestrian experience and contribute to the character, unity and identity of the neighbourhood.
- The design, materials and finishes of site furniture and pedestrian walkways shall be selected and should be generally consistent throughout the neighbourhood.
- Street and site furniture shall be durable and have a low life-cycle cost; be selected to discourage vandalism and use for skateboard activity; and be designed to meet the needs of a wide range of users including children, seniors, and those with disability.
- On-site utilities shall be architecturally integrated into the development or screened from view through a combination of hard and/or soft landscaping.
- Landscape planting within residential and commercial areas shall use minimum 50% native plantings with appropriate character, and mixed with other non-invasive plants.

- Use of materials such as permeable paving to maximize surface permeability to the greatest extent possible and practical is encouraged.
- The following surface treatments shall be incorporated into on-site hard surfaces and/or walkway design as a substitute for conventional pavement in low traffic areas.
- Porous pavement in areas with low-risk of ground water contamination. Porous pavements may be applied to lanes/access roads, driveways, and low-traffic parking areas.



- Concrete grid / modular pavers in low-traffic areas and may be applied to lanes/access roads, driveways, and low-traffic parking areas, footpaths and bike paths.



- Grass Pave/Grasscrete/Golpha plastic reinforcement products used in conjunction with gravel or grass surfaces.



- Curb cuts may be used to divert runoff from road surfaces into swales or rainwater gardens which contribute to evapotranspiration.



- Multi-use trails shall be incorporated into the development to promote pedestrian and cyclist activity and link to the surrounding trail network.

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28/09/15

Parking Lot Landscaping

- Screen at-grade and structured parking or service areas located within a residential building from the public street through such treatments as soft and hard landscaping elements. Where possible, parking should be integrated into the building structure or provided below grade.
- If surface parking areas are required in multi-unit residential buildings, place them away from public view and not between the public street and the building. Design landscape parking areas so they do not detract from any rear yard amenity space.
- Provide a landscape buffer along the edges of multi-unit residential parking areas, in situations where they are along a public street. Provide breaks in the buffers to connect the sidewalk to walkways on the site. Buffers may include low shrubs, trees, and decorative fences.
- In addition to trees and landscaping around the perimeter of surface parking areas, shade trees and landscaping are required within parking lots as per Section 111 of Zoning Bylaw 1987 No. 2500 Landscape Requirements, as amended from time to time.
- Pedestrian connections should be facilitated throughout the development, including through parking lots, and to adjoining land uses.

Parking and Traffic/Pedestrian Circulation

- Pedestrian connections shall be provided throughout the development, including through parking lots, and to adjoining land uses. Ornamental paving materials (stamped and coloured concrete or better) are required for all pedestrian connections.
- Provision of underground parking is encouraged and shall be designed with CPTED principles. Access to either underground or structured parking should be from a lane if possible.
- Parking shall be provided in enclosed and secured garages attached to individual units or in a secure underground parkade.
- Tandem parking on all end units is not permitted.
- Surface parking should be provided for in a number of smaller areas rather than one large lot, and shall be located primarily in the interior of the site or otherwise screened from view. Surface parking shall integrate landscaping and other design elements to reduce the massing of parking areas. Vehicular entrances to surface parking areas shall be landscaped, not gated, to create a subtle boundary between the semi-private and public areas.
- Private driveway access over greenways should be consolidated and minimized to ensure maximum safety of the users of the greenway. Private driveways accessing arterial roads may be restricted.
- Wheelchair access shall be provided throughout the development.
- Pedestrian connections shall be clearly visible, landscaped, and provided with hard surfaces suitable for older people and wheelchairs.
- Highly detailed paving materials are required along the High Street to indicate such things as storefronts, thru traffic, seating areas, and aesthetic relief areas.



- Without compromising the safety of users, all surface parking shall be visually screened from sub-neighbourhood streets through a combination of building arrangement and landscaping.
- At grade frontage shall be for commercial uses with only the access and egress points visible from the street.
- Vehicular access and egress points shall be combined and the presence and appearance of garage entrances should be designed so that they do not dominate the street frontage of a building.
- Horizontal floor designs are preferred to allow for adaptive reuse. Scissored floor designs are discouraged.
- Garage entrances shall have less prominence than the pedestrian entrances. A separate pedestrian entrance to the garage shall be provided. . This may be achieved through:
 - The relative importance of the garage entrance reduced by enhancing the pedestrian entrance.
 - Locating the entry on the side of the facade where it will draw less attention than if it is centered on the facade.
 - Recessing the portion of the facade where the entry is located to help conceal it.
 - Extending portions of the structure over the garage entry to help conceal it.
 - Emphasizing other elements of the facade to reduce the visual prominence of the garage entry.
 - Use of screening and landscaping to soften the appearance of the garage entry from the street.
 - Locating the garage entry where the topography of the site can help conceal it.
- Pedestrian entrances shall be separate from vehicular access points.
- Uses near access and egress points shall include design elements that reduce conflict between uses.
- Ramps to additional levels are to be contained within the structure and screened from view.
- Access and egress points shall have consistent sidewalk texture, colours and material for that portion intersecting with the streetscape.
- Provide separate parking areas for residential and commercial uses.
- The façade shall be architecturally integrated into the building and otherwise screened from view. Green walls and planters may be used in combination with architectural integration for this effect.
- Setbacks from above the second storey shall be incorporated into the design.
- If rooftop parking is provided additional landscaping shall be required consistent with Parking Lot Landscaping. Planters with arbors shall be provided continuously along the parapet.
- Lighting on the exterior (including the roof) shall be consistent with the entire building. Ornamental lighting shall be used on the rooftop parking areas.
- Public and private parking shall be clearly identified through a system of numbering and signage.

5101
28/09/15

- Underground parking structures shall be planned for the convenience and safety of users; shall have walls and ceilings finished in a light coloured paint for reflectivity; and shall incorporate motion-activated lighting to the greatest extent permitted.
- Adequate secured, sheltered and screened bicycle parking be provided on-site for short term and long term bicycle parking/storage facilities.
 - Short term bicycle parking should be in well-lit locations and clearly visible from a main building entrance and/or public roads with bicycle racks made of sturdy, theft-resistant material that is securely anchored to the floor or ground.
 - Longer term bicycle storage areas provided (secured in a separate room/enclosed area) as part of a parking structure should be located close to elevators and access points.

4825
30/05/11

4.1.2.4 TOWNHOUSES/ROWHOUSE

These guidelines are in addition to section 4.1.2.3 and apply to all townhouse and rowhouse developments.

Site Design

- Ground-oriented developments shall be designed with continuity in the design with respect to the exterior finishing materials and architectural detailing. Individual or paired units shall be significantly visually differentiated from other adjoining units (i.e. staggering in plan or elevation, varying rooflines, variation in exterior materials, variation in colour treatments, and architectural detailing).
- Developments which include multiple buildings on the same site shall include significant variation in the exterior design, façade, roofline articulation, material and colour of buildings.
- Units shall be oriented towards public roads, street greenways, natural areas, and greenlinks/commons where applicable.
- Scale building height and massing in proportion to open spaces.
- A pedestrian connection shall be provided from each development site to adjacent streets, street greenways, or public spaces. Public, semi-public and private space shall be clearly delineated.
- Tandem parking on end units is not permitted.

Landscaping

- Where there are multiple buildings on a site, buildings should be located to enclose courtyards and other landscaped spaces.

4825
30/05/11

4.1.2.5 APARTMENTS

These guidelines are in addition to section 4.1.2.3 and apply to all apartment developments.

Architectural Details

- Street facing facades of free-standing apartment buildings shall be designed for a pedestrian scale with the first storey architecturally differentiated from upper floors.
- The apparent mass of a building shall be reduced through roof design, facade articulation and shadowing.
- Incorporate a 'good neighbour' policy by ensuring building heights being stepped or terraced to relate to adjacent buildings.
- Building height and massing shall be in proportion to adjacent open space.

- The main entrance of each apartment building should include an awning, canopy, porte-cochere or other architectural element to provide protection from the weather.
- Orient the main building entrance to the street and provide a secondary building entrance and pedestrian link to adjacent municipal greenspace where applicable.
- Avoid blank or undifferentiated facades.
- Provide weather protection from parking area to front entrance where appropriate.

Landscaping

- Where there are multiple buildings on a site, buildings should be located to provide common space such as courtyards.

Parking and Traffic/Pedestrian Circulation

- Minimize above grade projection of parking structures.
- Provide drop-off areas at grade level near the main building entrance where possible.
- Provide resident parking underground or within the building.



9 September, 2020

Joel Nagtegaal
Development Planner
Township of Langley
20338 65 Avenue
Langley, BC V2Y 3J1

Re: Development Application Project 08-26-0208 1 1124300 BC LTD/ISLE OF MANN

CIVIC: 20701 - 84 Avenue and 8400 Block of 207A Street

LEGAL: Lot C Plan EPP35087 and West Half Lot 10 Plan 3434; both of Section 26 Township 8 NWD

We have reviewed the above proposal.

We calculate the approximate number of students generated by this proposal will be as follows:

Type of Housing	Number of Units	Elementary K-5	Middle 6-8	Secondary 9-12
Single Family Units	10	4	1	3
Duplex	6	2	1	2
Rowhomes	5	2	1	1

Given the current school catchments this development would impact Willoughby Elementary School, Yorkson Creek Middle School and R.E. Mountain Secondary School. As you know, while the Langley School District is not responsible for the amount or pace of development we work closely with the Township of Langley in order to advocate to the Ministry of Education for the development of joint sites to benefit our students.

We make every effort to keep students in their catchment schools, but if there is insufficient space in the catchment school we will find them a space at another school in the district.

Please advise if you need any other information.

Yours sincerely,

Brian Iseli, CPA, CMA
Secretary Treasurer

Joel Nagtegaal
Planner
Township of Langley
203-65th Ave.
Langley, BC V2Y 3J1

October 5, 2018

Dear Mr. Nagtegaal

RE: 20701 – 84th Ave. (08-26-0208)

The following is a summary of the Public Information Meeting held October 4, 2018 at the Willoughby Community Hall, 20809- 83rd Ave. Langley. Local residents were notified of the meeting via a mailed invitation to 480 surrounding households which was done on September 19, 2018. An invitation to the meeting was also made via an advertisement in the September 20th & 27th editions of the Langley Advance newspaper.

The meeting commenced at 6:30 pm and was attended by approximately 12 local residents as recorded on the sign-in sheet.

Brent Tedford of the IOM Property Group and Kasel Yamashita for McElhanney Consulting Services were in attendance to answer residents. Ruby Sandher from the Township of Langley Planning Dept. attended in an observer capacity.

Five residents provided comments on the forms available at the meeting:

- Two expressed concerns over sidewalks and bike lanes
- Three expressed concerns related to density
- Three wanted to ensure on street parking was not allowed.
- One resident wanted to ensure on street parking would be allowed

We note that the project will have sidewalks and bike lanes conforming to the Township standard. Whether or not on street parking is allowed is an operational decision for the Township. The project density is in conformance with the Yorkson NCP.

I have attached a copy of the sign in-sheet and the comments forms. Also attached is a copy of the display boards used at the meeting.

The meeting was concluded at 8:00pm.

We believe the light attendance at the meeting is an indication that the vast majority of the local residents do not have concerns about the project, and we look forward to bringing this project forward to Council.

Sincerely,

Brent Tedford
Senior Development Manager

Sign-In Sheet

Project: 20701 84th Avenue

Meeting Date: October 4, 2018

Facilitator: Isle of Mann

Place/Room: Willoughby Community Hall

Print Name

Address

Signature

FOIPPA s.22(1)

1.

FOIPPA s.22(1)

FOIPPA s.22(1)

2.

FOIPPA s.22(1)

3.

FOIPPA s.22(1)

4.

FOIPPA s.22(1)

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We Appreciate Your Input

Isle of Mann Property Group has applied to rezone & subdivide 20701-84th Avenue & LT C, SEC 26, TWP 8, NWD PL EPP35087 from Suburban Residential Zone SR-2 to Residential Zones R-CL(B), R-CL(A), R-1(A), R-CL(SD) and R-CL(RH) to allow development of a 28-lot mixed use residential subdivision.

Please return your completed comment form to the comment box before you leave. Thank you.

Please Tell Us About Yourself

Name: FOIPPA s 22(1)
 Address: FOIPPA s 22(1)
 Phone: FOIPPA s 22(1) Email: _____

Let Us Know Your Comments or Questions Regarding the Application

We live in the townhouse complex
 across the street - ~~and~~ We are concerned
 about the density and parking as
 this is already an issue. We're wondering
~~what~~ if thought has been given to human
 traffic for sidewalks et. with the
 middle school nearby.

We Appreciate Your Input

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Please return your completed comment form to the comment box before you leave. Thank you.

Please Tell Us About Yourself

Name: FOIPPA s 22(1)
 Address: FOIPPA s 22(1)
 Phone: FOIPPA s 22(1) Email: FOIPPA s 22(1)

Let Us Know Your Comments or Questions Regarding the Application

WE WOULD LIKE TO SEE THE WHOLE ACREAGE
 DEVELOPED AT ONCE & NOT PIECE MEAL.
 CONSTRUCTION WILL TAKE IT'S TOLL ON PEOPLE
 ALREADY LIVING IN THE AREA.
 WOULD LIKE TO SEE LESS DENSITY.

We Appreciate Your Input

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Please return your completed comment form to the comment box before you leave. Thank you.

Please Tell Us About Yourself

Name: FOIPPA s 22(1)
 Address: FOIPPA s 22(1)
 Phone: FOIPPA s 22(1) Email: FOIPPA s 22(1)

Let Us Know Your Comments or Questions Regarding the Application

OPPOSED TO INCREASE DENSITY
 BEYOND THE CURRENTLY-APPROVED
 PLAN BY THE TOWNSHIP.

THE INCREASE WILL ONLY MAKE THE
 PARKING WORSE.

We Appreciate Your Input

Isle of Mann Property Group has applied to rezone & subdivide 20701-84th Avenue & LT C, SEC 26, TWP 8, NWD PL EPP35087 from Suburban Residential Zone SR-2 to Residential Zones R-CL(B), R-CL(A), R-1(A), R-CL(SD) and R-CL(RH) to allow development of a 28-lot mixed use residential subdivision.

Please return your completed comment form to the comment box before you leave. Thank you.

Please Tell Us About Yourself

Name: Cynthia Hamilton, Willoughby Residents Assn
 Address: POB 32095 Langley Vm2m3
 Phone: 6048886606 Email: Willoughby.residents.assn@gmail.com

Let Us Know Your Comments or Questions Regarding the Application

We would like to see consistency
in the ~~lot~~ sidewalks & bike lanes
All parking should be off road
with appropriate covenant requiring
off street parking. "No stopping" signs
on 84 (& 207A! not just no parking)

We Appreciate Your Input

Isle of Mann Property Group has applied to rezone & subdivide 20701-84th Avenue & LT C, SEC 26, TWP 8, NWD PL EPP35087 from Suburban Residential Zone SR-2 to Residential Zones R-CL(B), R-CL(A), R-1(A), R-CL(SD) and R-CL(RH) to allow development of a 28-lot mixed use residential subdivision.

Please return your completed comment form to the comment box before you leave. Thank you.

Please Tell Us About Yourself

Name: FO PPA s.22(1)

Address: FO PPA s.22(1) Langley, B.C.

Phone: FO PPA s.22(1) Email: FOIPPA s.22(1)

Let Us Know Your Comments or Questions Regarding the Application

We need sidewalks, bike lanes, and plenty of parking. Houses are getting smaller and parking on the road is always, and already full. Hoping everyone parks in their garage is wishful thinking.